

5.49 DUPLEXES

5.49.1 DEFINITION

Duplexes shall be defined as two-family dwelling units within a single structure on a single lot, and containing separate cooking and bathing facilities. Both dwelling units shall be entirely contained under the same roof and constructed to comply with applicable building and fire codes.

5.49.2 STANDARDS

5.49.2.1 Duplexes shall be permitted in RA, SN-9, and UN-6 zoning districts.

5.49.2.2 All lot sizes and dimensional standards found in Table 4.7-1 in this Ordinance shall apply to residential structures containing duplexes as though they were detached single-family dwellings.

5.49.2.3 A minimum of one and a half (1.5) off-street parking space is required for each dwelling unit located within a duplex, for a total of three (3) total required parking spaces.

5.49.2.4 Duplexes shall be site-built or modular residential structures. Manufactured homes shall not be utilized as duplexes, except in the RA zoning district.

5.49.2.5 Accessory Dwelling Units (ADUs) shall not be permitted on parcels developed with a duplex, except where permitted as multifamily development, as that would constitute a third dwelling unit.

5.50 TOWNHOUSES

5.50.1 DEFINITION

Townhouses are single-family residential structures comprised of three or more dwelling units constructed in a series, group, or row of attached units separated by property lines and with a yard on at least two sides. Townhouses may be accessed from a public street at the front of the structure or from a rear lane or alley.

5.50.2 DEVELOPMENT STANDARDS

Townhomes shall comply with all density and dimensional standards set forth in Article 4 of this Ordinance, including the following:

TABLE 5.50-1: SETBACKS FOR TOWNHOUSES (ATTACHED SINGLE-FAMILY DWELLINGS)

Minimum Lot Size	Minimum Lot Width	Minimum Lot Depth	Minimum Front Setback	Minimum Rear Setback	Minimum Side Setback	Minimum Side (Corner) Setback	Minimum Distance Between Townhome Structures
2,000 SF	20FT	100FT	20FT	10FT	0ft or 5ft*	15FT	20FT

*See §5.50.2.2

Figure 1: Minimum Building Setbacks for Townhouse Structures within a Sequence of Townhouses

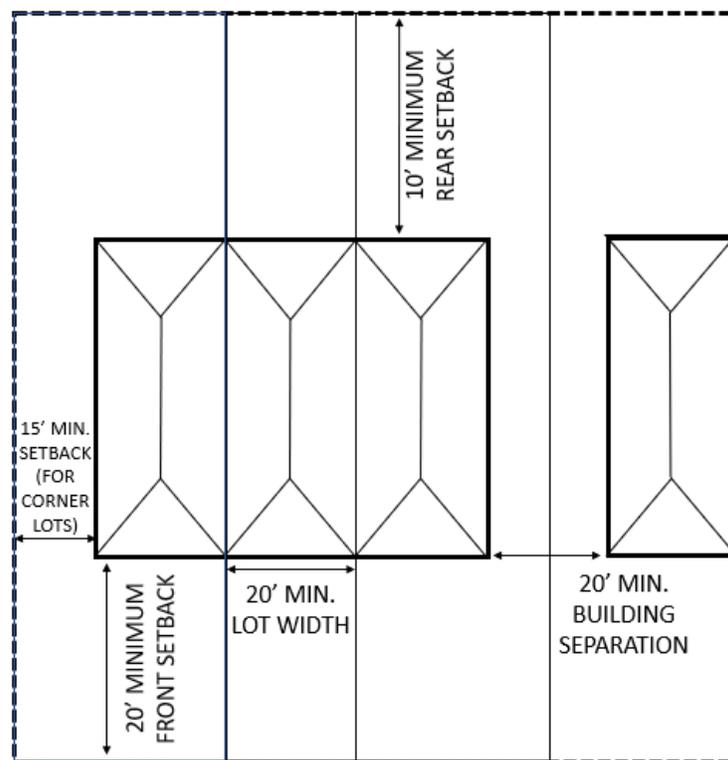
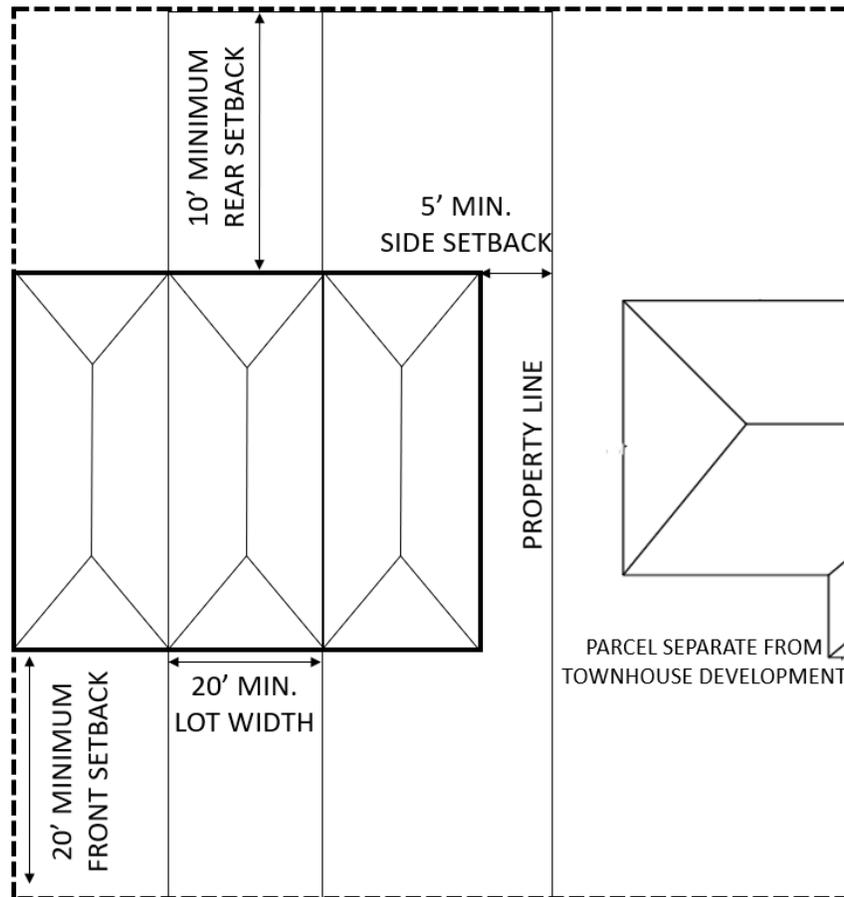


Figure 2: Minimum Building Setbacks for Townhouse Structures Abutting Parcels Outside of the Associated Development

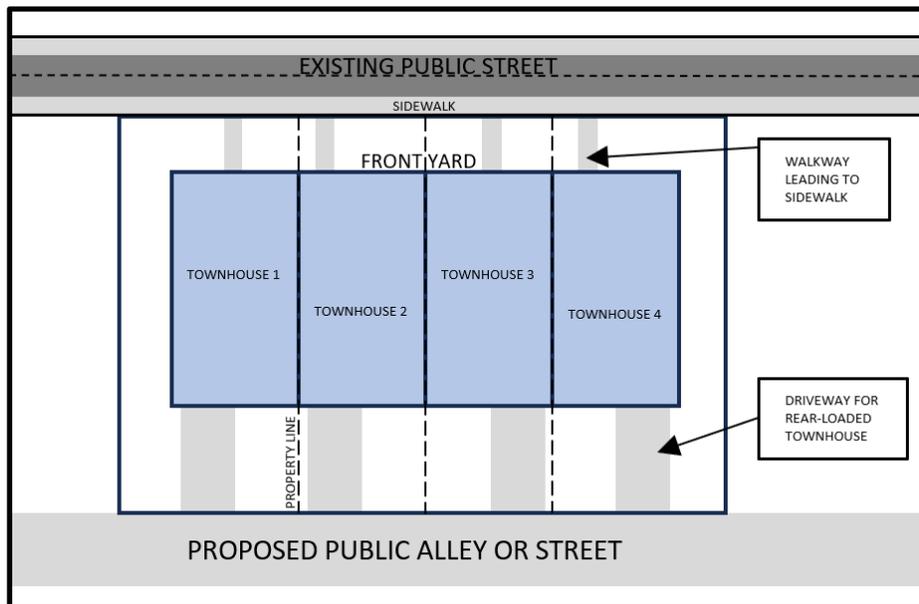


5.50.2.1 Townhouses shall have a minimum front setback of 20ft and a minimum rear setback of 10ft. When townhouses front two or more public rights-of-way, the side or corner minimum building setback(s) shall be 15ft, as illustrated in Figure 1.

5.50.2.2 Regarding minimum side building setbacks, townhouses necessitate a 0ft side setback when arranged in a row of multiple attached dwellings within a single structure. However, when abutting another parcel that is not directly related to the townhouse development, a 5ft minimum building setback is required along that shared side property line, as illustrated in Figure 2. This is primarily intended for infill development in which a row of townhouses may be developed adjoining an existing structure, and access to a side yard is necessary for the placement of mechanical equipment or other appurtenances, or space may be required for exterior maintenance or backyard access.

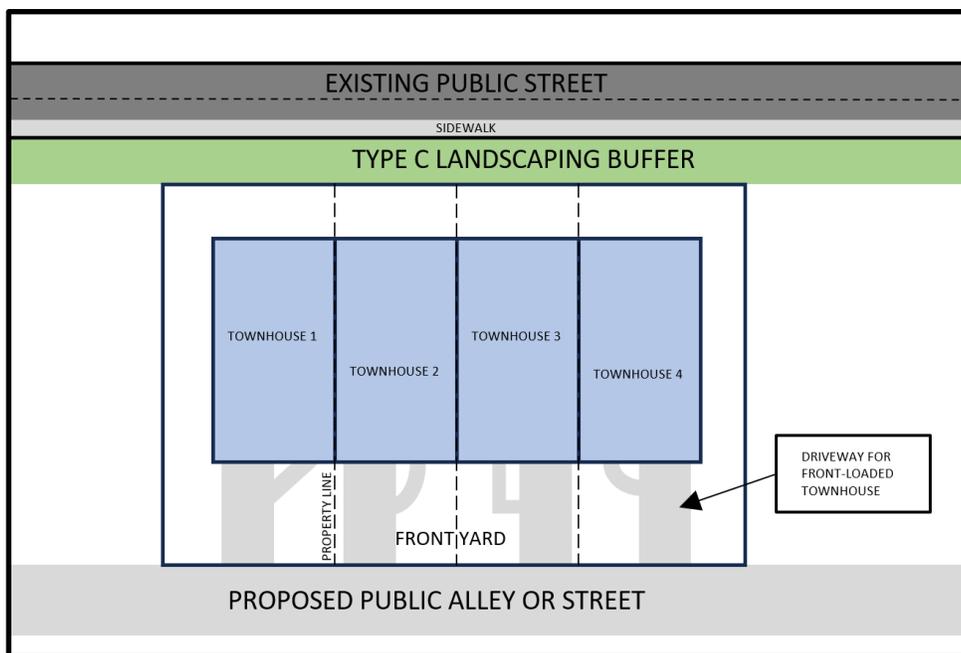
5.50.2.3 Townhouses proposed along existing public rights-of-way are strongly encouraged to orient the structure(s) to front the existing right-of-way and incorporate rear-loaded garages into the townhouse design when practical, as illustrated in Figure 5.48-1.

Figure 3: TOWNHOUSE PLACEMENT FRONTING AN EXISTING PUBLIC RIGHT-OF-WAY



5.50.2.4 When not practical to front townhouses along an existing thoroughfare, the townhouse structures may be oriented with the rear of the townhouses facing an existing right-of-way. A Type C landscaping buffer shall be required to screen the backyards of the townhouses from view along the right-of-way, as illustrated in Figure 5.48-2. Refer to Table 7-4 in Article 7 of this Ordinance for specific Type C buffer yard standards. The Type C landscaping buffer shall be dedicated as common open space and may count towards a development’s required passive open space.

Figure 4: TOWNHOUSE PLACEMENT BACKING UP TO AN EXISTING RIGHT-OF-WAY



5.50.3 VEHICULAR ACCESS

Townhouses may have vehicular access via a public street (front-loaded townhouse) or public alley (rear-load). Townhome access via a private street shall not be a permitted. Public streets and alleys shall conform to standards of the appropriate jurisdiction's technical specifications for street construction.

Alleys should be designed in such a manner to facilitate interconnectivity and permit through traffic and avoid stubs or dead-ends where practical. Alleys that bisect entire blocks to serve rear-loaded townhouses are encouraged where practical.

5.50.4 SUPPLEMENTAL PARKING STANDARDS

For rear-loaded townhouses, two standard off-street parking spaces shall be required for every townhouse unit (see Table 8-1 in §8.3 of this Ordinance). For front-loaded townhouses, two standard off-street parking spaces shall be required for every dwelling unit, as well as one supplemental parking space for every four dwelling units. The supplemental parking spaces may be provided in the form of a standard off-street parking space within a designated parking lot or, if permitted by the appropriate jurisdiction's Public Works Department or equivalent agency, clearly delineated, striped, on-street parking along proposed new public streets. When proposing a townhouse development with on-street supplemental parking, the appropriate jurisdiction's technical specifications for public streets shall be incorporated into the design.

5.50.4.1 OFF-STREET PARKING STANDARDS

Off-street parking may be provided as supplemental parking spaces for townhouse developments, provided that they are in compliance with parking lot standards put forward in Article 8 of this Ordinance. The following additional standards shall apply to supplemental off-street parking for townhouse developments:

- Off-street parking lots shall contain no more than 14 parking spaces.
- Off-street parking lots shall be reasonably distributed throughout the townhouse portion(s) of a development so as to be in convenient proximity to dwellings.
- Off-street parking lots shall be paved and curbed.
- Off-street parking lots shall be accessible for pedestrians by a minimum 5ft-wide sidewalk that connects to the development.
- Off-street parking lots shall not abut existing public streets unless screened from view of the street with a Type C landscape buffer (see Table 7-4 in Article 7 of this Ordinance).
 - Exceptions to this requirement may be granted by the Zoning Administrator or his/her designee where there are spatial and practical limitations for screening of parking lots, such as infill projects in dense urban neighborhoods.